

STD LOWI 0800z

Depart Innsbruck and climb to FL180 and route via RTT, BETOS, and OBAXA (or as appropriate to ATC) to NORIN;

At NORIN, continue southwards thru points "A", "B", and "C";

At point "C", conduct two circuits thru diamond pattern (points "C", "D", "E" and back to "C"), {outside turns - 270 degree - at each corner} [first circuit at FL180 and second circuit at FL150];

Upon return to "C", turn towards ICAO point ELTAR;

At appropriate stage conduct Profile Descent (1000ft/min) to reach 1000ft AGL over Lake Garda;

Upon reaching vicinity of ELTAR, conduct Profile Climb (1000ft/min) back to FL150 (north/south over Lake Garda, as necessary) ending at point "F";

At point "F", continue thru point "G" to point "C".

From point "C", repeat Diamond pattern ["C", "D", "B", "E", "C"];

Upon return to point "C", turn northward to point "A";

Recover to Innsbruck.

ETA LOWI 1130z.

REFUEL

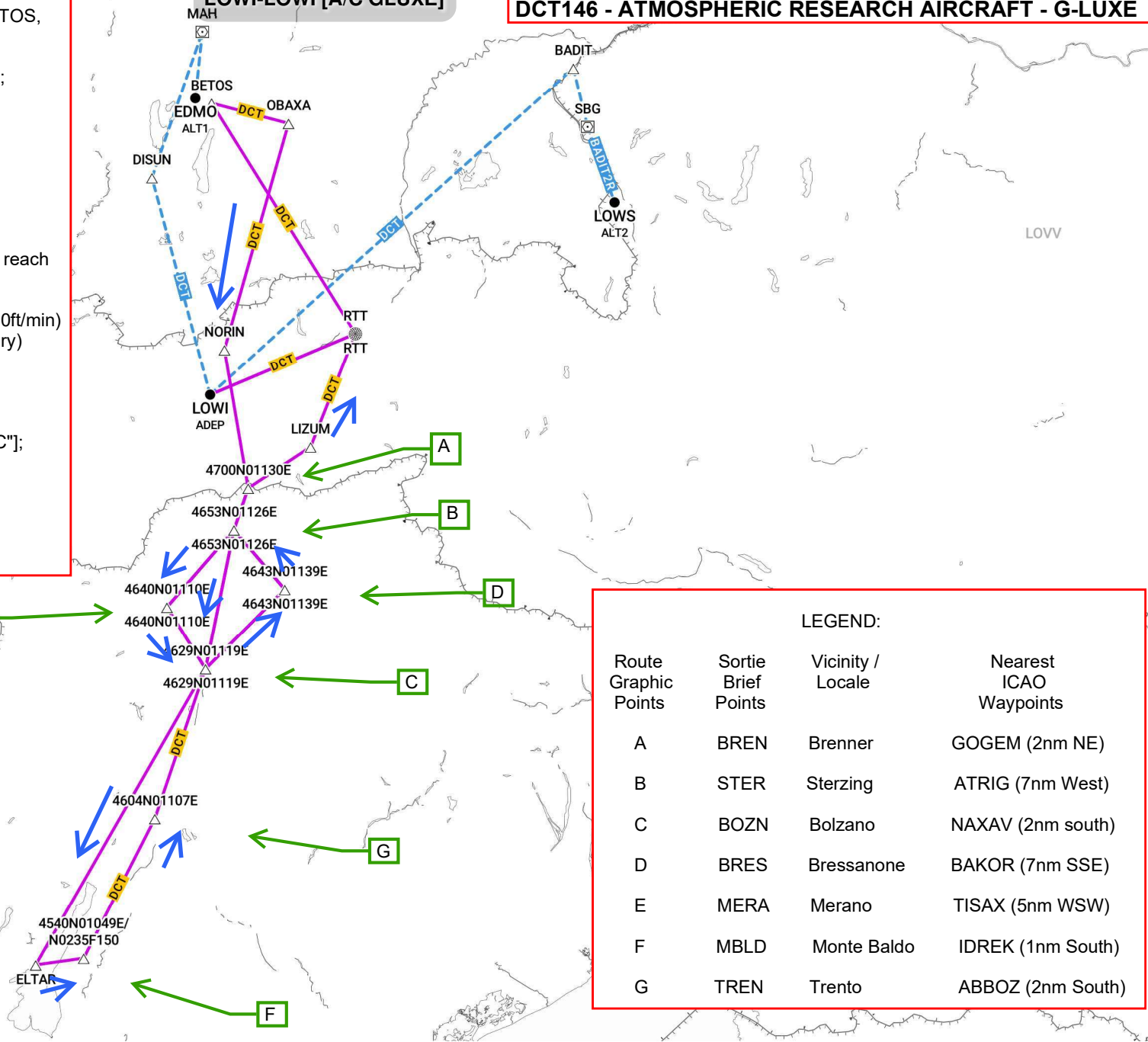
STD LOWI 1300z

Repeat of the above scenario.

ETA LOWI 1630z.

Wednesday, 30 July 2025 (AM & PM)
DCT146 - ATMOSPHERIC RESEARCH AIRCRAFT - G-LUXE

LOWI-LOWI [A/C GLUXE]

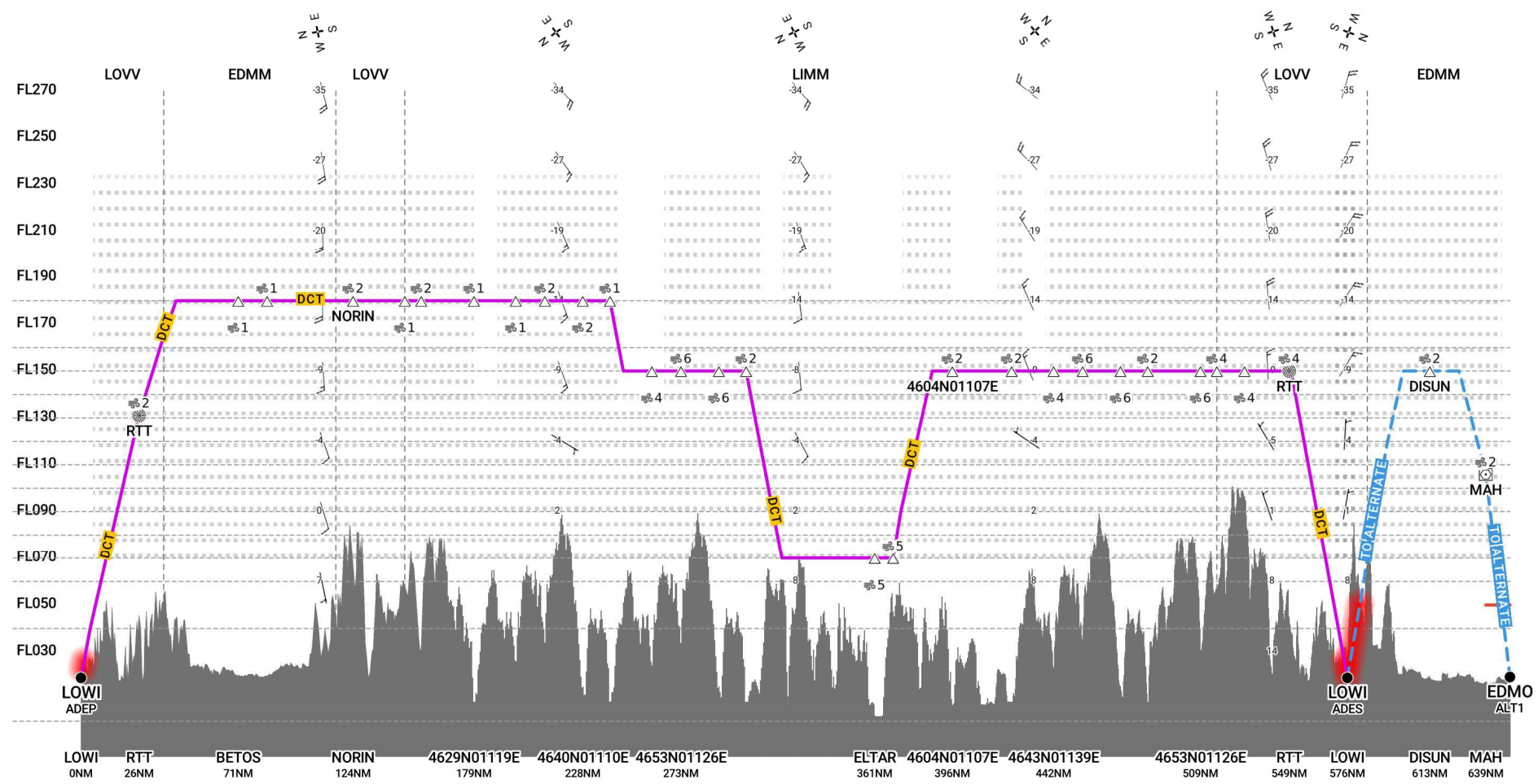


LEGEND:

Route Graphic Points	Sortie Brief Points	Vicinity / Locale	Nearest ICAO Waypoints
A	BREN	Brenner	GOGEM (2nm NE)
B	STER	Sterzing	ATRIG (7nm West)
C	BOZN	Bolzano	NAXAV (2nm south)
D	BRES	Bressanone	BAKOR (7nm SSE)
E	MERA	Merano	TISAX (5nm WSW)
F	MBLD	Monte Baldo	IDREK (1nm South)
G	TREN	Trento	ABBOZ (2nm South)



LOWI-LOWI [A/C GLUXE]



Route	FIR boundary	Clear Air Turbulence Scale
SID/STAR	Airway Label	In Cloud Turbulence Scale
MEA	EDTO Markers	Caution, no data available
Route to ALT/TALT	SEOPS Glide Cone Coverage	Icing Scale
Tropopause	Direction of Wind	Wind Speed
Terrain	Wind shear	
Caution: terrain data not available for Greenland.	Direction of Travel	
	Performance Envelope	

